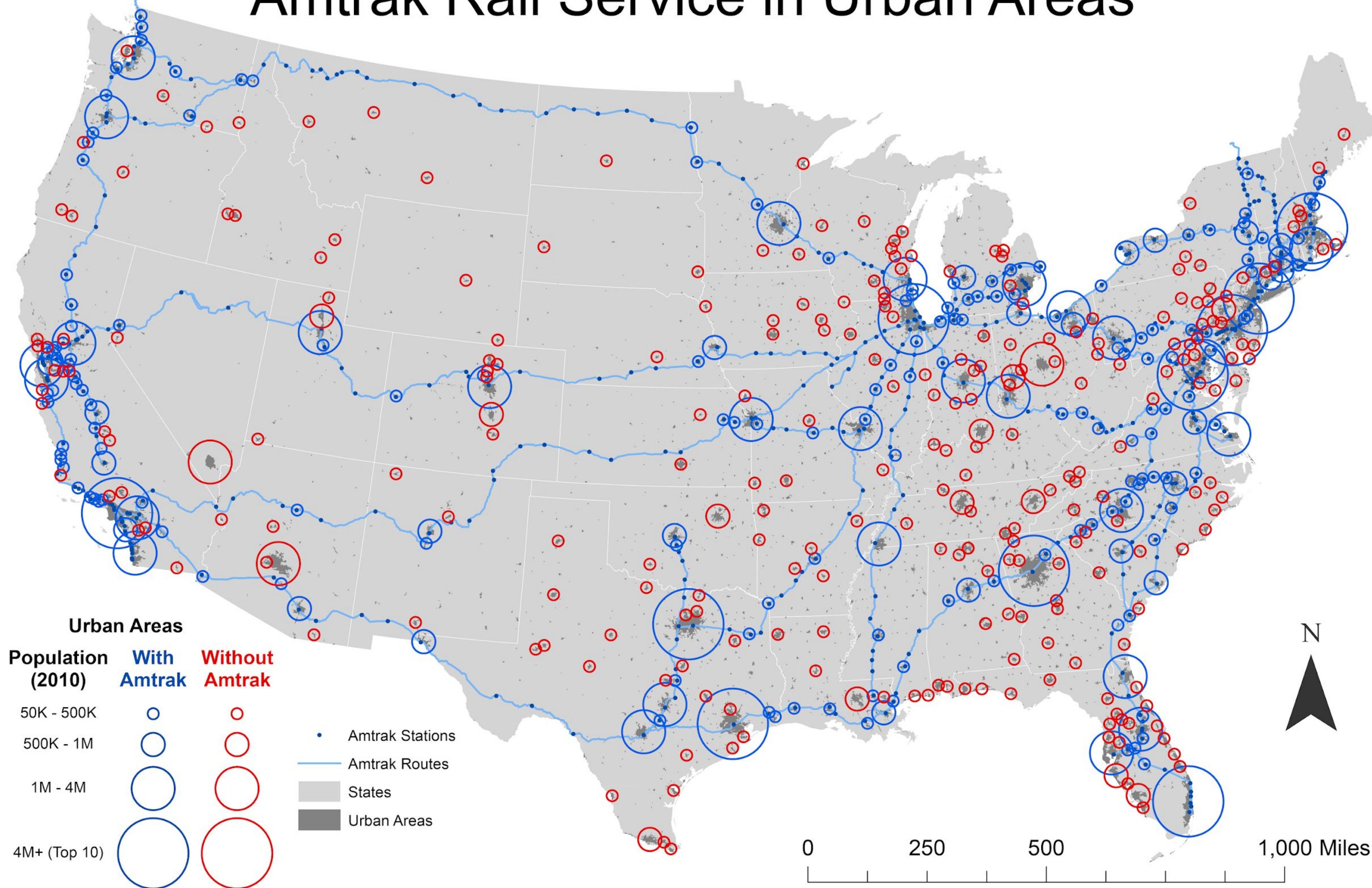




The State of Amtrak in Three Maps

By Miles Churchill

Amtrak Rail Service in Urban Areas

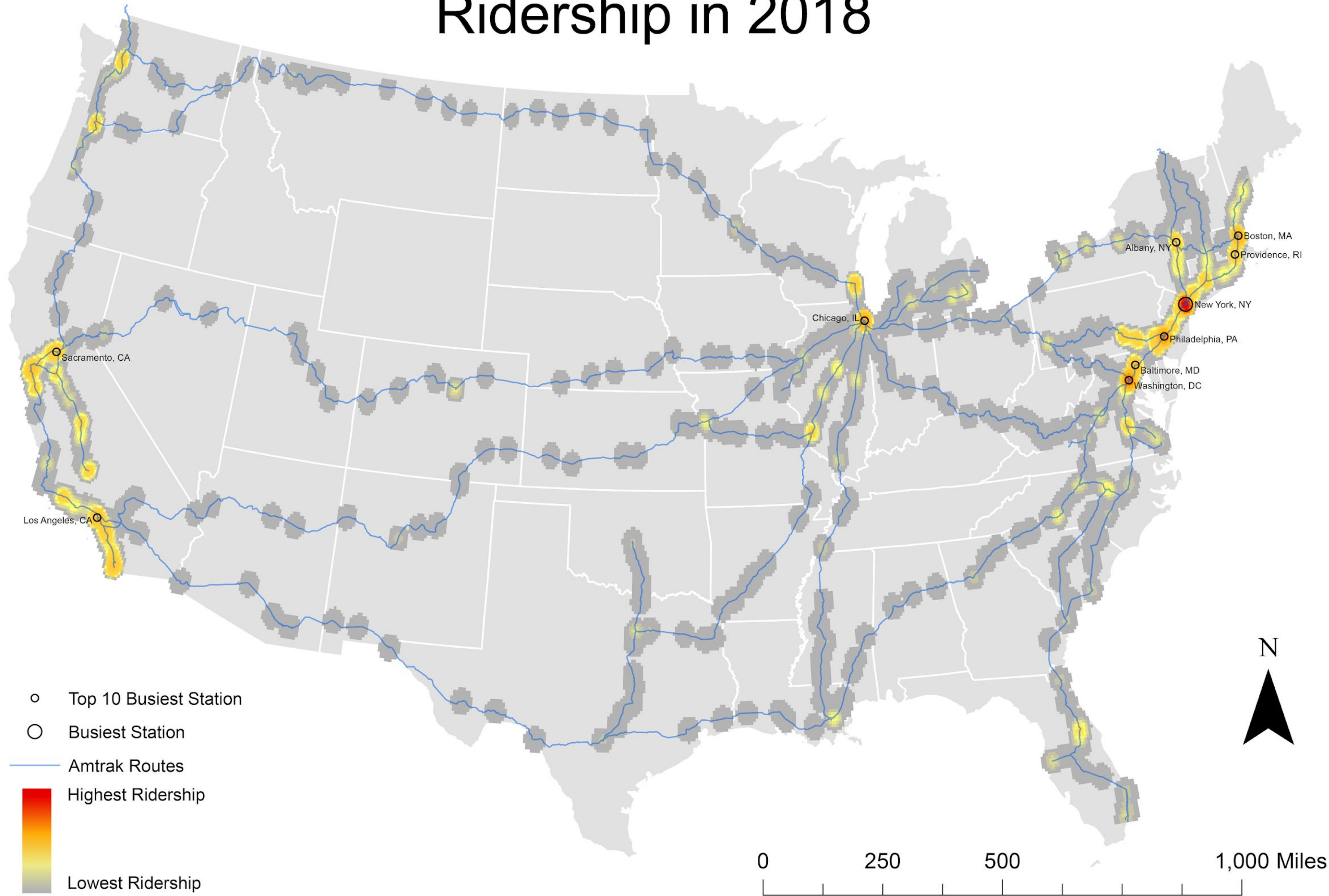


Amtrak is the nation's intercity passenger rail service which was created by Congress in 1971 to take over the failing private passenger rail services. Amtrak's mandate is to "provide efficient and effective intercity passenger rail mobility consisting of high-quality service that is trip-time competitive with other intercity travel options." Amtrak serves many of the country's largest urban centers through over 21,000 miles of routes, however, there are many cities which are not covered by the rail network. Urban areas not served by Amtrak are highlighted in the map on the left by red circles, while urban areas which are served by Amtrak are shown in blue. This map is intended to show the areas of the continental US which are left behind by the rail network, and highlight the potential for expansion and providing more connectivity between urban areas of all sizes. This map does not include Amtrak Thruway bus services, which account for only a small amount of Amtrak ridership.

Top 10 Urban Areas Without Amtrak

| Name | Population (2010) |
|-------------------------------------|-------------------|
| Phoenix--Mesa, AZ | 3,629,114 |
| Las Vegas--Henderson, NV | 1,886,011 |
| Columbus, OH | 1,368,035 |
| Louisville/Jefferson County, KY--IN | 972,546 |
| Nashville-Davidson, TN | 969,587 |
| McAllen, TX | 728,825 |
| Dayton, OH | 724,091 |
| Allentown, PA--NJ | 664,651 |
| Tulsa, OK | 655,479 |
| Sarasota--Bradenton, FL | 643,260 |

Ridership in 2018



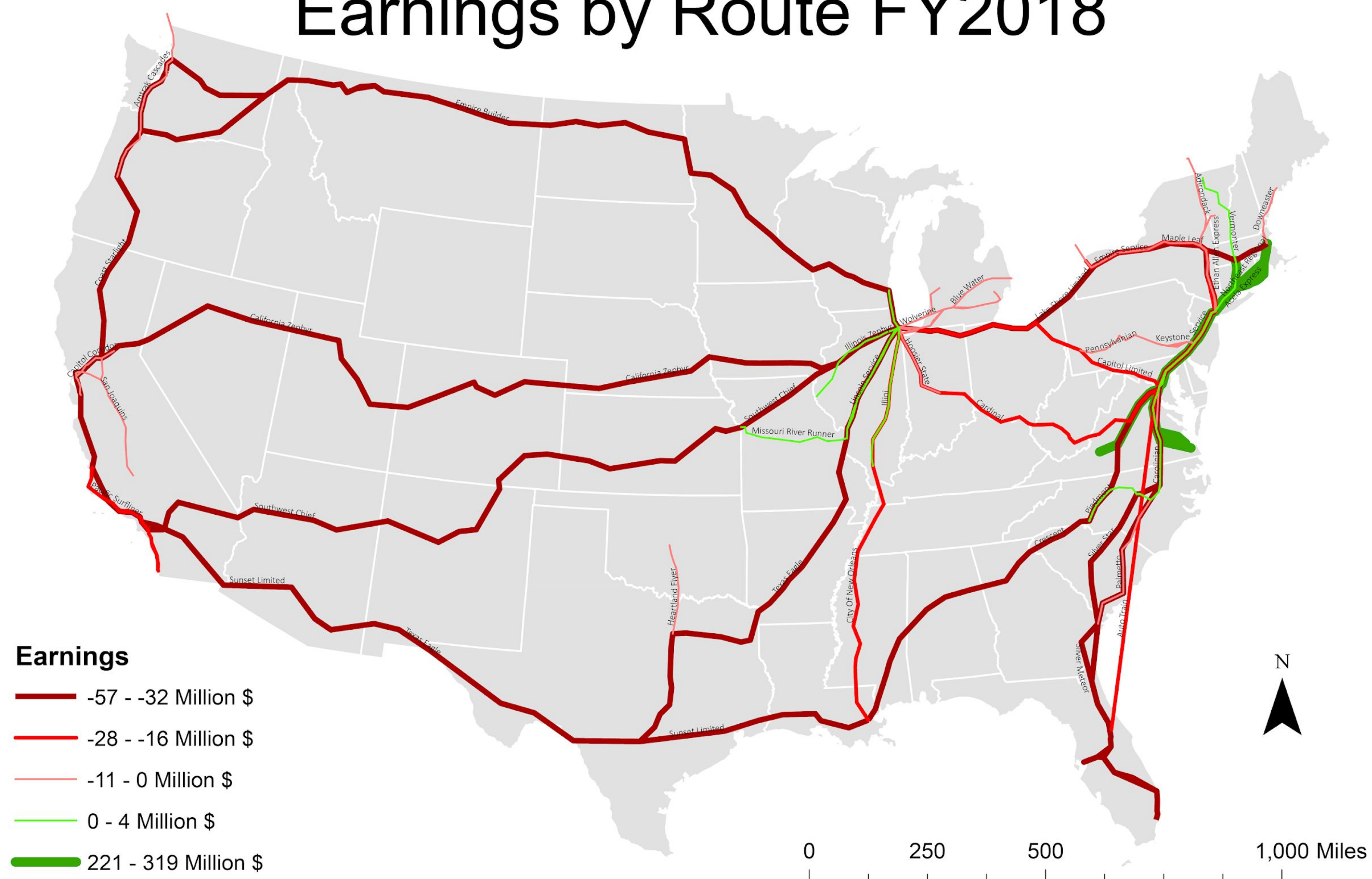
Amtrak was used for approximately 31 Million trips in 2018. The map at left displays a heatmap of where those trips started and ended in the network. Cities with more reliable, frequent and faster services (such as the Northeast Corridor from Boston to Washington, DC) see by far the highest ridership, while long distance routes (often with services only a few times a week) see the lowest.

Top 10 Stations by 2018 Ridership

| Station | Ridership |
|----------------------|-----------|
| New York, NY | 9,860,366 |
| Washington, DC | 5,039,100 |
| Philadelphia, PA | 4,416,987 |
| Chicago, IL | 3,293,590 |
| Boston, MA | 1,533,543 |
| Los Angeles, CA | 1,446,853 |
| Sacramento, CA | 1,072,063 |
| Baltimore, MD | 1,026,004 |
| Albany-Rensselaer, N | 790,547 |
| Providence, RI | 758,375 |

This final map and table displays the financial performance of the various rail services offered by Amtrak. Each route is colored and sized according to the earnings or loss made by the route. The most profitable routes for Amtrak are its Northeast Corridor Services. Many of the shorter, state supported routes serving the Midwest, Northwest, and East Coast are either mildly profitable or require a low amount of subsidy while the long distance routes typically lose the most money. Some have proposed that Amtrak eliminate long distance routes and focus on regional travel. The proposal faces pushback from those who say the long distance routes play an important role for rural communities and states.

Earnings by Route FY2018



| Amtrak Routes FY 2018 Performance | | | | | | |
|-----------------------------------|---------------------|-----------------------|-----------------------|------------------------|-------------------------|---------------------|
| Category | Route | Revenue (Millions \$) | Expense (Millions \$) | Earnings (Millions \$) | Ridership (Millions \$) | On Time Performance |
| North East Corridor | Acela Express | 624.3 | 305.5 | 318.8 | 3428.3 | 82% |
| | Northeast Regional | 767.7 | 546.6 | 220.9 | 9812.9 | 78% |
| | Adirondack | 12.9 | 13.4 | -0.5 | 111.7 | 70% |
| | Amtrak Cascades | 61.2 | 67.2 | -6.0 | 806.1 | 58% |
| | Blue Water | 11.8 | 12.8 | -1.0 | 185.0 | 67% |
| | Capitol Corridor | 61.6 | 70.3 | -8.7 | 1706.8 | 90% |
| | Carolinian | 23.0 | 19.2 | 3.7 | 256.9 | 55% |
| | Empire Service | 48.1 | 65.6 | -17.4 | 1150.5 | 78% |
| | Ethan Allen Express | 4.7 | 5.2 | -0.6 | 49.7 | 87% |
| | Heartland Flyer | 6.1 | 6.9 | -0.8 | 68.1 | 44% |
| | Hiawatha Service | 23.5 | 21.7 | 1.8 | 844.4 | 93% |
| | Hoosier State | 3.7 | 5.6 | -1.9 | 27.9 | 77% |
| | Illini | 17.5 | 17.1 | 0.4 | 245.9 | 28% |
| | State Supported | Illinois Zephyr | 15.5 | 14.6 | 0.9 | 191.6 |
| Keystone Service | | 51.3 | 56.0 | -4.7 | 1519.9 | 87% |
| Lincoln Service | | 39.9 | 32.2 | 7.7 | 586.2 | 68% |
| Maple Leaf | | 29.5 | 33.6 | -4.1 | 366.7 | 75% |
| Missouri River Runner | | 15.5 | 14.3 | 1.1 | 169.5 | 83% |
| Pacific Surfliner | | 111.1 | 128.9 | -17.8 | 2946.0 | 77% |
| Pennsylvanian | | 14.7 | 17.3 | -2.6 | 214.8 | 72% |
| Pere Marquette | | 6.3 | 7.0 | -0.7 | 95.5 | 77% |
| Piedmont | | 6.4 | 7.4 | -1.1 | 167.2 | 59% |
| San Joaquin | | 84.0 | 95.1 | -11.1 | 1078.9 | 77% |
| Downeaster | | 17.5 | 17.4 | 0.1 | 540.0 | 63% |
| Vermont | | 11.4 | 10.7 | 0.8 | 97.9 | 71% |
| Wolverine | | 30.7 | 32.6 | -1.9 | 483.7 | 59% |
| Long Distance | | Auto Train | 72.0 | 87.5 | -15.5 | 224.8 |
| | California Zephyr | 59.1 | 116.3 | -57.3 | 418.2 | 52% |
| | Capitol Limited | 21.4 | 49.0 | -27.6 | 219.0 | 31% |
| | Cardinal | 7.1 | 22.6 | -15.5 | 96.7 | 54% |
| | City of New Orleans | 19.4 | 40.4 | -21.0 | 237.8 | 70% |
| | Coast Starlight | 44.5 | 90.1 | -45.6 | 417.8 | 65% |
| | Crescent | 33.1 | 72.6 | -39.4 | 274.8 | 14% |
| | Empire Builder | 57.6 | 109.5 | -51.9 | 428.9 | 57% |
| | Lake Shore Limited | 28.4 | 64.1 | -35.7 | 337.9 | 42% |
| | Palmetto | 30.9 | 36.3 | -5.4 | 387.9 | 48% |
| | Silver Meteor | 40.1 | 79.9 | -39.8 | 337.0 | 43% |
| | Silver Star | 34.8 | 69.7 | -34.9 | 368.5 | 32% |
| | Southwest Chief | 45.5 | 102.7 | -57.2 | 331.2 | 55% |
| | Sunset Limited | 12.4 | 47.6 | -35.2 | 97.1 | 49% |
| Texas Eagle | 25.7 | 57.5 | -31.8 | 335.8 | 46% | |